

Lake County TRANSPORTATION IMPROVEMENT Project

NEWS NEWS NEWS

The Lake County Transportation

Improvement Project

(LCTIP) is jointly sponsored
by the Illinois Department of
Transportation and the Illinois
State Toll Highway Authority.

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- ▶ LCTIP Develops
Wide Range of
Potential Solutions
- ▶ What's The Next
Step in The Process?
- ▶ Other Proposals
- ▶ Public Meeting #1
Questions and
Comments

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REMINDER:

Our Baseline Improve-
ments include
improvements expected
to be built by 2020.
Already, 46 miles (of
the total 74 miles)
of expected roadway
improvements are
either under construc-
tion or funded in the
next 5 years.

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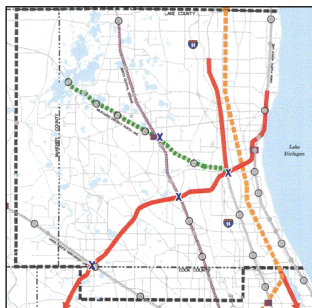
LCTIP DEVELOPS WIDE RANGE OF POTENTIAL SOLUTIONS

The Lake County Transportation Improvement Project (LCTIP) has achieved another project milestone—development of the initial roadway and transit improvements. The nine roadway improvement packages and the set of rail and bus improvements establish the range of upgrades to be considered by the LCTIP for evaluation. These improvements were developed based on a comprehensive evaluation of the transportation needs in Lake County conducted earlier this year.

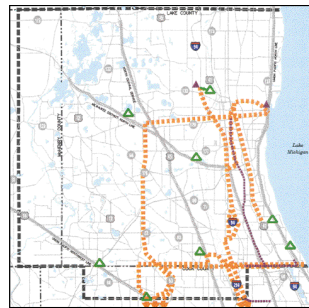
The LCTIP worked closely with Metra, Pace and RTA staff to develop transit proposals that represent reasonable projects for Lake County (see Figures 1-3). The types of improvements identified for consideration for the rail system include commuter rail service expansions, signal improvements, and consolidation of freight service. Improvements for bus service include express services and corridors for additional service. The proposals also include improvements that provide better links between modes. These improvements represent opportunities to enhance transit as part of an overall solution.

...continued on page 4

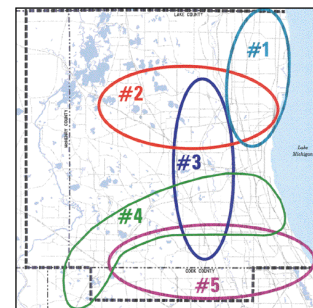
**FIGURE 1. POTENTIAL RAIL
IMPROVEMENTS**



**FIGURE 2. POTENTIAL EXPRESS
SERVICES AND TRANSFER CENTERS**



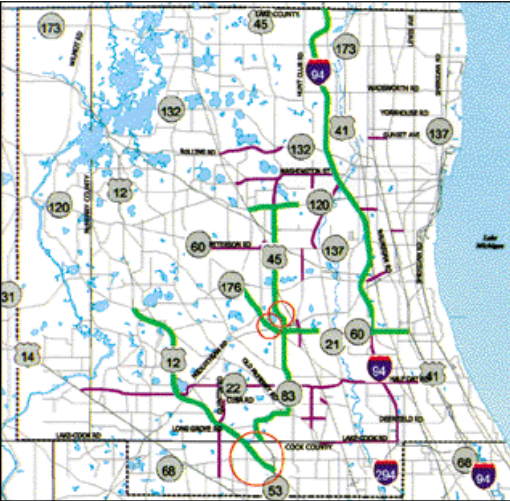
**FIGURE 3. CORRIDORS FOR ADDITIONAL
BUS SERVICE**



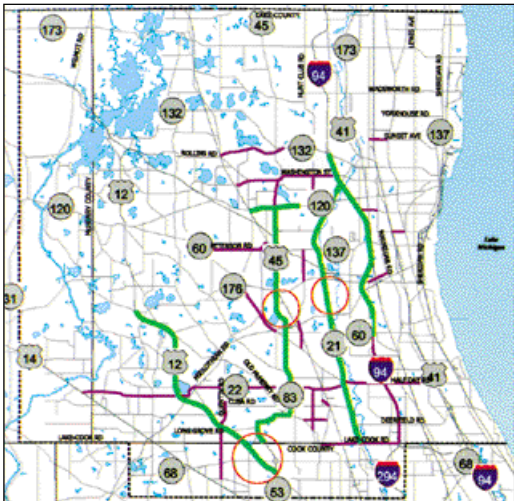
POTENTIAL RAIL IMPROVEMENTS LEGEND	
■	New Commuter Rail Services
■	Consolidation of Freight Service
■	Signal Improvements
X	Rail Line Transfers
■	Baseline Improvements

POTENTIAL EXPRESS SERVICES AND TRANSFER CENTERS LEGEND	
■	Express Services
▲	Transfer Centers
■	Baseline Improvements

FIGURE 4. I-94 SET OF IMPROVEMENTS



**FIGURE 5. IL 83/US 45 SET OF IMPROVEMENTS
(WITH US 12)**



**FIGURE 6. IL 83/US 45 SET OF IMPROVEMENTS
(WITH IL 120 BYPASS)**

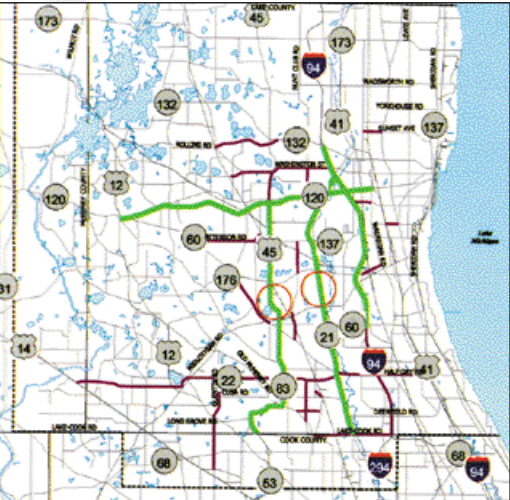


FIGURE 7. IL 53 SET OF IMPROVEMENTS (FREEWAY)

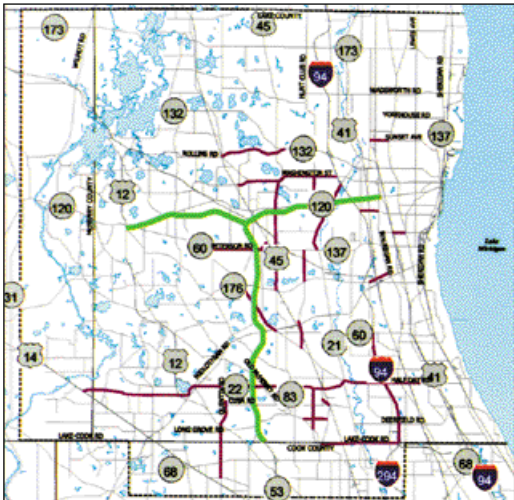


FIGURE 8. IL 53 SET OF IMPROVEMENTS (TOLLWAY)

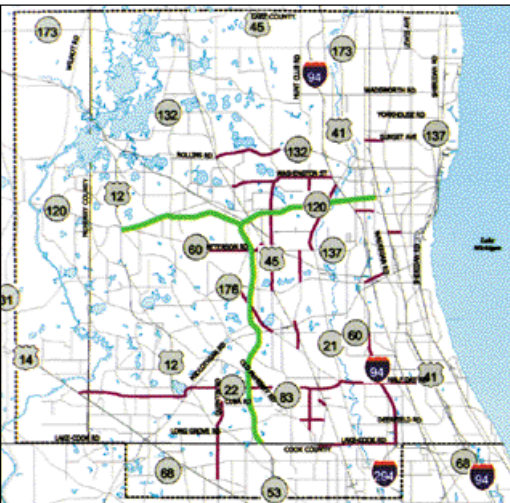
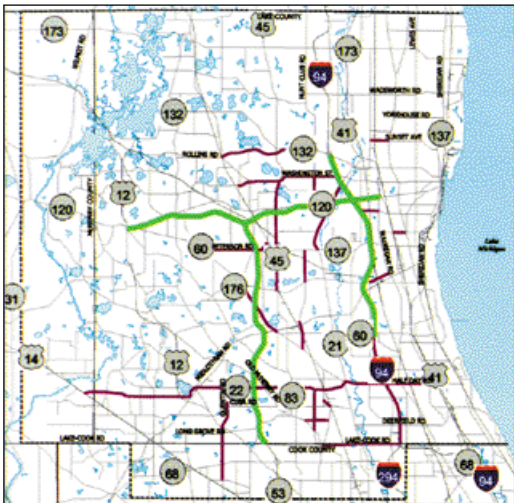


FIGURE 9. IL 53 SET OF IMPROVEMENTS (ARTERIAL)

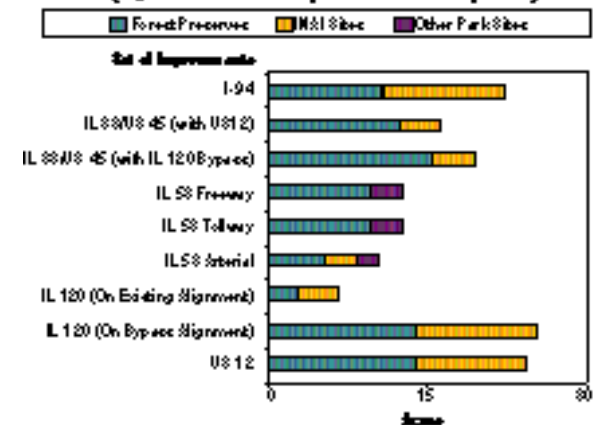
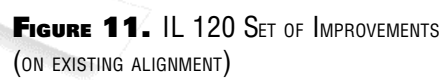


MAP LEGEND FOR ROAD IMPROVEMENTS

Proposed Improvements

Baseline Improvements

Bypass Consideration



LCTIP DEVELOPS WIDE RANGE OF POTENTIAL SOLUTIONS (CONTINUED FROM PAGE 1)

Our roadway proposals are in addition to the Baseline Improvements. Doing just the Baseline Improvements is not enough, given existing congestion levels and the projected quarter million new residents in Lake County by 2020. We can't solve all the problems, so we focused on the worst congestion and the most effective combinations using a structured, computer-aided approach. Our nine roadway improvement sets are shown in Figures 4-12.

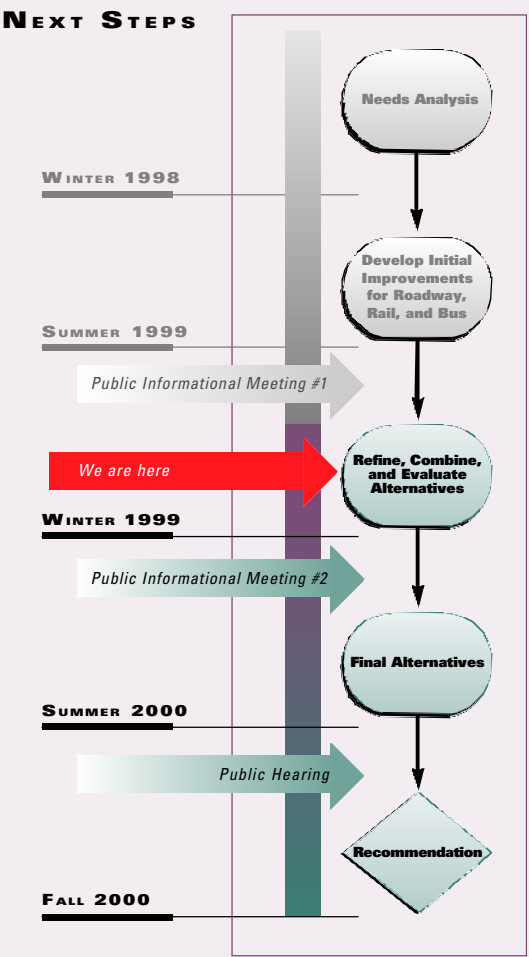
In August we presented these proposals in a series of public informational meetings held throughout Lake County. More than 800 people attended the events to review the proposed improvements, ask questions, and provide input. The overwhelming majority of participants agreed that major transportation improvements are needed in one form or another. The comments received have been reviewed and will assist us in identifying and addressing issues and concerns as we move into the next phase of the process—refining and combining the roadway, rail, and bus improvements to form complete alternatives and then evaluating the alternatives.

WHAT'S THE NEXT STEP IN THE PROCESS?

Through each successive stage of development, the alternatives will be increasingly refined and detailed. In the next step of the process, we will further develop the alternatives by:

- Identifying interchange locations
- Refining roadway footprints to avoid or minimize impacts
- Refining roadway and rail alignments and bus routes
- Identifying the feeder road system needed to support the major road improvements

Then we will analyze and compare the alternatives using transportation, environmental, societal, and financial evaluation criteria. Our findings will be summarized and presented in another series of public informational meetings for review and comment. We will continue to work with the public and others as we have throughout the project.

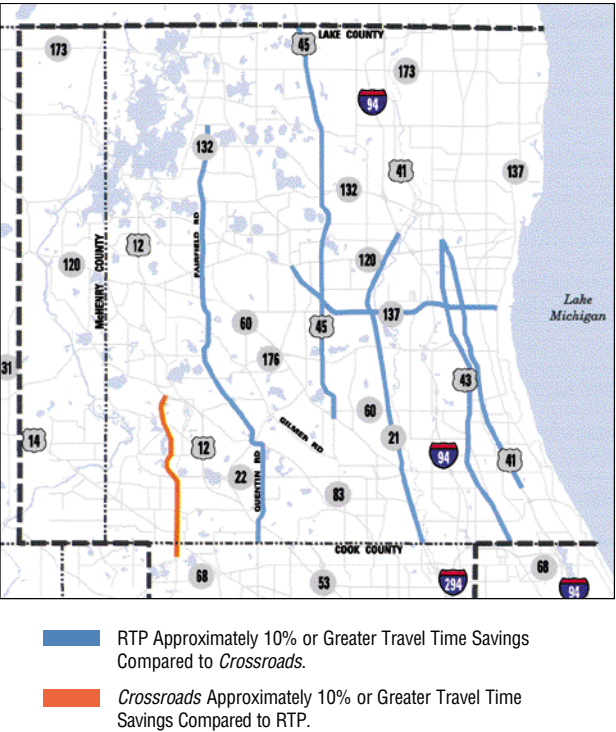


OTHER PROPOSALS... CROSSROADS

The Environmental Law and Policy Center (ELPC) and Citizens Organized for Sound Transportation (COST) have proposed limited existing roadway improvements and the addition of some rail service as the solution for Lake County's transportation needs in a document titled *Crossroads: Smart Transportation Options for Lake County*. They claim that implementing these improvements would lead to greater congestion relief when compared to the endorsed Regional Transportation Plan (RTP).^{*} Their analysis misused the 2020 population forecasts, resulting in inaccurate conclusions.

Using state-of-the-art techniques, we correctly analyzed the *Crossroads* proposal and compared it to the regionally endorsed 2020 transportation plan. We found that the 2020 transportation plan improves travel times by about 10% on 100 miles of major roadways, whereas the *Crossroads* proposal improves travel times by the same margin on only 8 miles of major roadways (see Figure 13). The *Crossroads* proposal is not as effective in reducing congestion levels and accommodating Lake County's future growth.

FIGURE 13. TRAVEL TIME COMPARISON 2020 RTP VS CROSSROADS



^{*}Regional Transportation Plan includes 21 major projects throughout northeastern Illinois, including the extension of Illinois Route 53 in Lake County.

WHAT HAPPENS IF WE IMPROVE ONLY EAST-WEST ROADS?

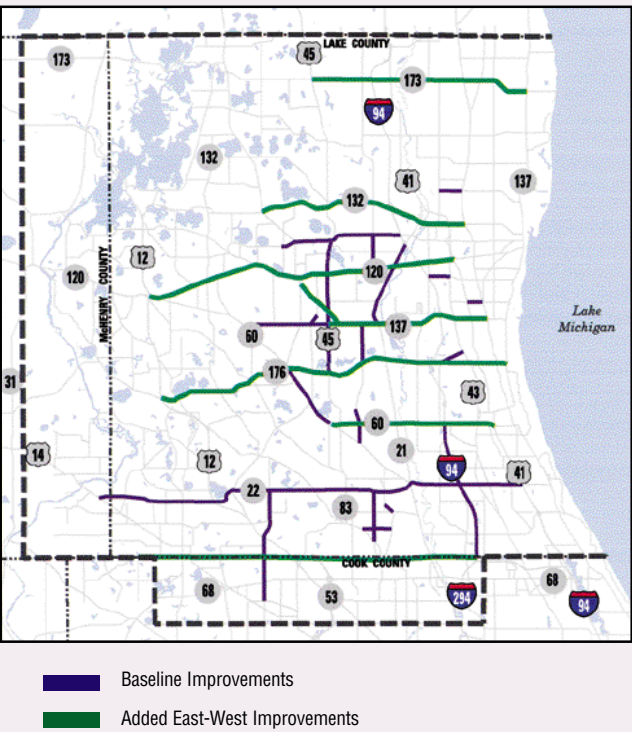
Some people have expressed the opinion that east-west roads are more congested and should be improved. The Lake County Transportation Improvement Project and other transportation providers in the area recognize that there are east-west travel needs in Lake County, and through a collaborative process identified more than 40 miles of east-west roadway improvements as part of the "Baseline Improvements." Improvements to Lake Cook Road, Illinois Route 22, Peterson Road, Washington Street, and Rollins Road are included in the Baseline. These improvements are being proposed regardless of the recommendations made by this project.

In response to comments, we developed and tested an "east-west" improvement scenario (see Figure 14). This scenario performed worse than any of our roadway improvement sets, achieving only 78% of our goal.

These results should not be surprising. An analysis of existing travel patterns shows that north-south travel is predominant and the system lacks sufficient north-south capacity. In developing our alternatives, we targeted the worst congested roadways and most efficient combinations

of improvements. All have a mix of north-south and east-west improvements.

FIGURE 14. EAST-WEST ROADWAY IMPROVEMENT SCENARIO



QUESTIONS AND COMMENTS

In addition to the strong turnout at the public informational meetings held this August, we received nearly 600 written and oral comments. In response to your input, we provide the following responses to the major recurring comments and questions.

A MAJORITY OF COMMENTERS SUPPORT MAJOR TRANSPORTATION IMPROVEMENTS.

The Lake County Transportation Improvement Project has identified a wide range of transportation improvement options to address the major transportation problems in Lake County. At the public informational meetings, nine different sets of roadway improvements as well as rail and bus improvements were presented. We received approximately 450 comments specifically about the roadway improvements. The following table summarizes how participants at the public informational meetings felt about the proposed sets of roadway improvements.

Set of Roadway Improvements	Percent
Support I-94 set of improvements	2%
Oppose I-94 set of improvements	0%
Support IL 83 / US 45 set of improvements	2%
Oppose IL 83 / US 45 set of improvements	1%
Support IL 53 set of improvements	56%
Oppose IL 53 set of improvements	33%
Support IL 120 set of improvements	1%
Oppose IL 120 set of improvements	0%
Support US 12 set of improvements	4%
Oppose US 12 set of improvements	1%
Do nothing (Baseline)	0%
Total	100%

THERE IS A HIGH DEGREE OF FRUSTRATION WITH EXISTING TRAFFIC CONGESTION. COMMENTERS WANT ROAD IMPROVEMENTS TO BE BUILT NOW.

We received many comments expressing concern about the time required for the highway development process. These studies are necessary to provide us with the technical basis to make good transportation decisions. Our intent with this project is to move through the process as efficiently as possible and have an answer to what should be done by the fall of 2000. As for implementing road improvements, projects to widen 46 miles of existing roads are ongoing or funded for construction in the next 5 years.

MANY WERE CONCERNED WITH THE FAILURE OF ROAD IMPROVEMENTS TO KEEP PACE WITH DEVELOPMENT.

Population and employment have increased rapidly over the last decade. During this same period, less than 5% of the major roads were improved. With the county adding 11,000 new residents per year, transportation has not been able to keep pace. Studies to address congestion are ongoing. In total, 74 miles of improvements to existing roads are anticipated by 2020 (LCTIP Baseline Improvements). These include improvements to IL Route 22, Lake Cook Rd., Pulaski Rd., Martin Luther King Dr., Midlothian Rd., Weiland Rd., Butterfield Rd., I-94, Sunset Ave., Bradley Rd., Buffalo Grove Rd., US 45, Peterson Rd., Hunt Club Rd., Rollins Rd., and Quentin Rd. Recommendations of the LCTIP would be in addition to these improvements.

MANY BELIEVE GROWTH WILL HAPPEN REGARDLESS OF ROAD IMPROVEMENTS.

Census data show that Lake County is growing despite a lack of major transportation improvements. Forecasts show that an additional 250,000 people will move to Lake County by 2020,

QUESTIONS AND COMMENTS (CONTINUED)

regardless of transportation improvements. Using a rigorous analysis, the population impact of any of the LCTIP alternatives would be less than 4% overall. The belief that Lake County will grow anyway is fostered by its historical growth patterns and geographic position in the metropolitan area. Transportation has not been, nor is it expected to be, the major driver of population growth in Lake County.

GET MORE OUT OF THE EXISTING TRANSPORTATION SYSTEM (I.E., TRAFFIC SIGNAL COORDINATION, ROAD UNDERPASS/OVERPASS AT RAILROAD CROSSINGS, AND ADDITIONAL TURNING LANES AT INTERSECTIONS).

We are considering transportation management strategies like traffic signal coordination as part of this project. It is important to note, however, that these types of strategies typically have limited impacts in terms of reducing overall traffic volumes, usually around 1 percent, and thus do not eliminate the need for major transportation improvements.

WOULD IL 53 CAUSE DEVELOPMENT AND GROWTH?

Transportation is a minor factor. Between 1980 and 1995 Lake County's population increased by 29%, or 132,000 residents. Over this same period, less than 5% of the roads were improved. Lake County's population is forecast to increase to 800,000 by the year 2020. The extension of IL 53 or other LCTIP alternatives would contribute less than 4% to population in the year 2020.

Local jurisdictions are responsible and have control over land use decisions. The provision of sewer and water infrastructure, and zoning, allow for development. We do our best to keep pace with these land use decisions and try to consider future growth.

IMPROVE EXISTING ROADS AND SEE WHAT HAPPENS.

Whether it's improvements to existing roads or new highways or both, it is obvious that as Lake County

continues to add population (projected quarter million increase by 2020), the need for major improvements becomes increasingly evident. Lake County can ill afford to take a "see what happens" approach. It is important that the right solution be pursued now, that best accommodates Lake County's future growth and transportation needs.

PROVIDE IMPROVEMENTS FOR ALTERNATIVE TYPES OF TRANSPORTATION (TRANSIT, BICYCLES, AND PEDESTRIANS).

We are taking a serious look at bus and rail improvements. The alternatives will include a combination of road improvements as well as transit components. The transit components will focus on enhancing services and increasing the number of people using transit to meet their travel needs. The rail and bus strategies under consideration include rail extensions of existing Metra service, rail station parking enhancements, and bus service expansion. Although we are evaluating major improvements to the transit network, currently nearly 90% of all work trips are made by automobile, whereas less than 5% are made by transit. Major improvements to the roads, which complement transit services, are necessary to meet Lake County's transportation needs.

ARE YOU CONSIDERING THE QUALITY OF ENVIRONMENTAL RESOURCES WHEN ASSESSING IMPACTS?

Yes, we will undertake a rigorous evaluation of the environmental impacts of the improvements in the next step of our process, when we begin to refine the improvements to avoid or minimize impacts to resources. We will evaluate the alternatives, using environmental, societal, financial, and transportation measures. Our evaluation will involve qualitative as well as quantitative values.

Lake County TRANSPORTATION IMPROVEMENT Project

IN THIS ISSUE:

- Initial Roadway and Transit Improvements
- Other Proposals
- Public Meeting #1 Questions and Comments

Project #10
Lake County
Transportation
Improvement
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FOR FURTHER INFORMATION
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